

WELCOME

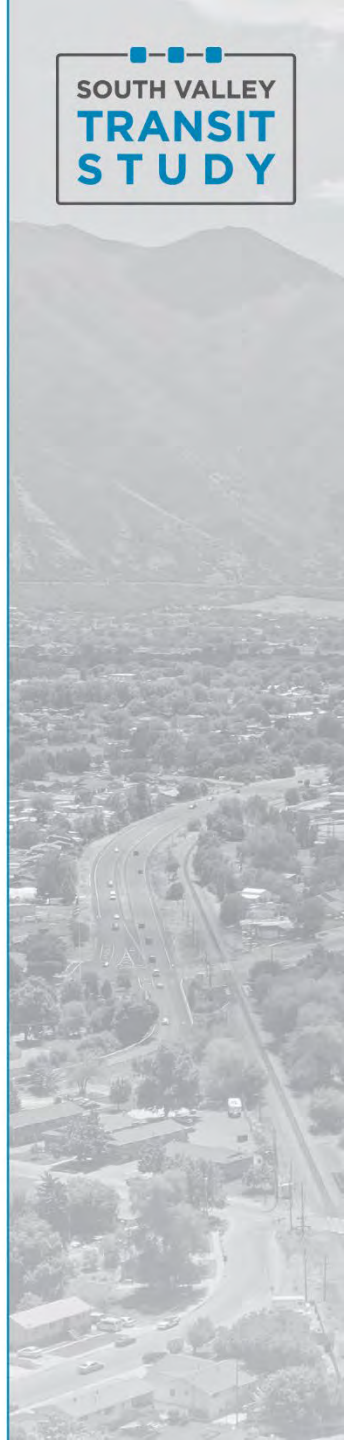
ONLINE PUBLIC MEETING

South Valley Transit Study

The meeting will begin shortly after 6 p.m. to allow the participants time to join the Zoom webinar. Please visit the study website at **southvalleytransit.com** to access further study details.

To submit questions, click the Q&A button on your screen. Our study team will be monitoring the questions and will answer as many as possible following the presentation.

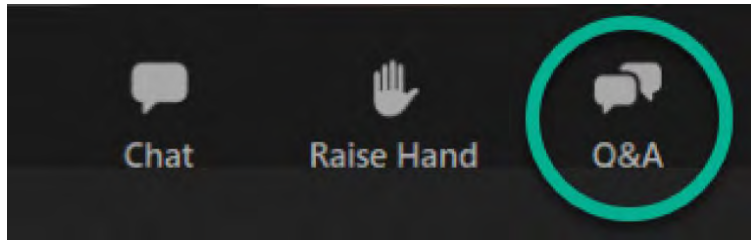
Please note: This event is being recorded and will be available for viewing at **southvalleytransit.com**.



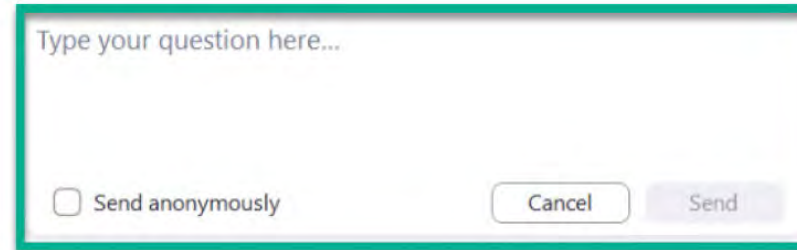
QUESTION AND ANSWER

1. TO SUBMIT A QUESTION

Click on the Q&A button at the bottom of the screen

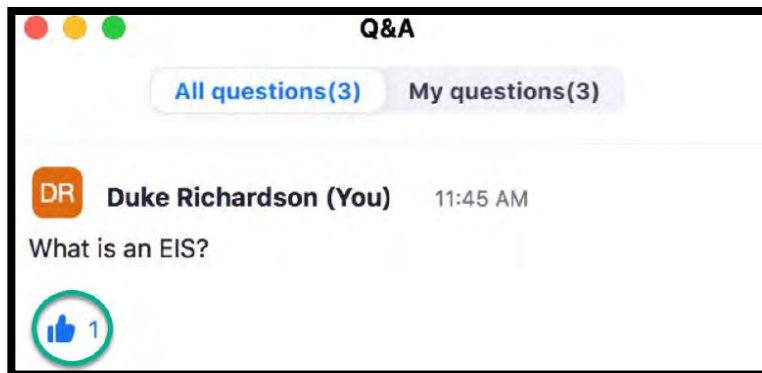


Type question in the Q&A Window



2. TO LIKE OR UPVOTE A QUESTION

Click on the “thumbs-up” button underneath questions



This helps us to know how many people have the same question

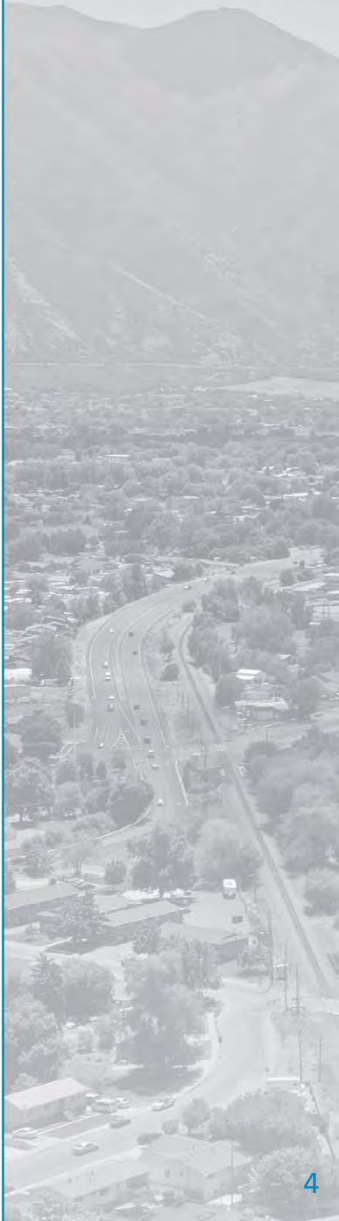
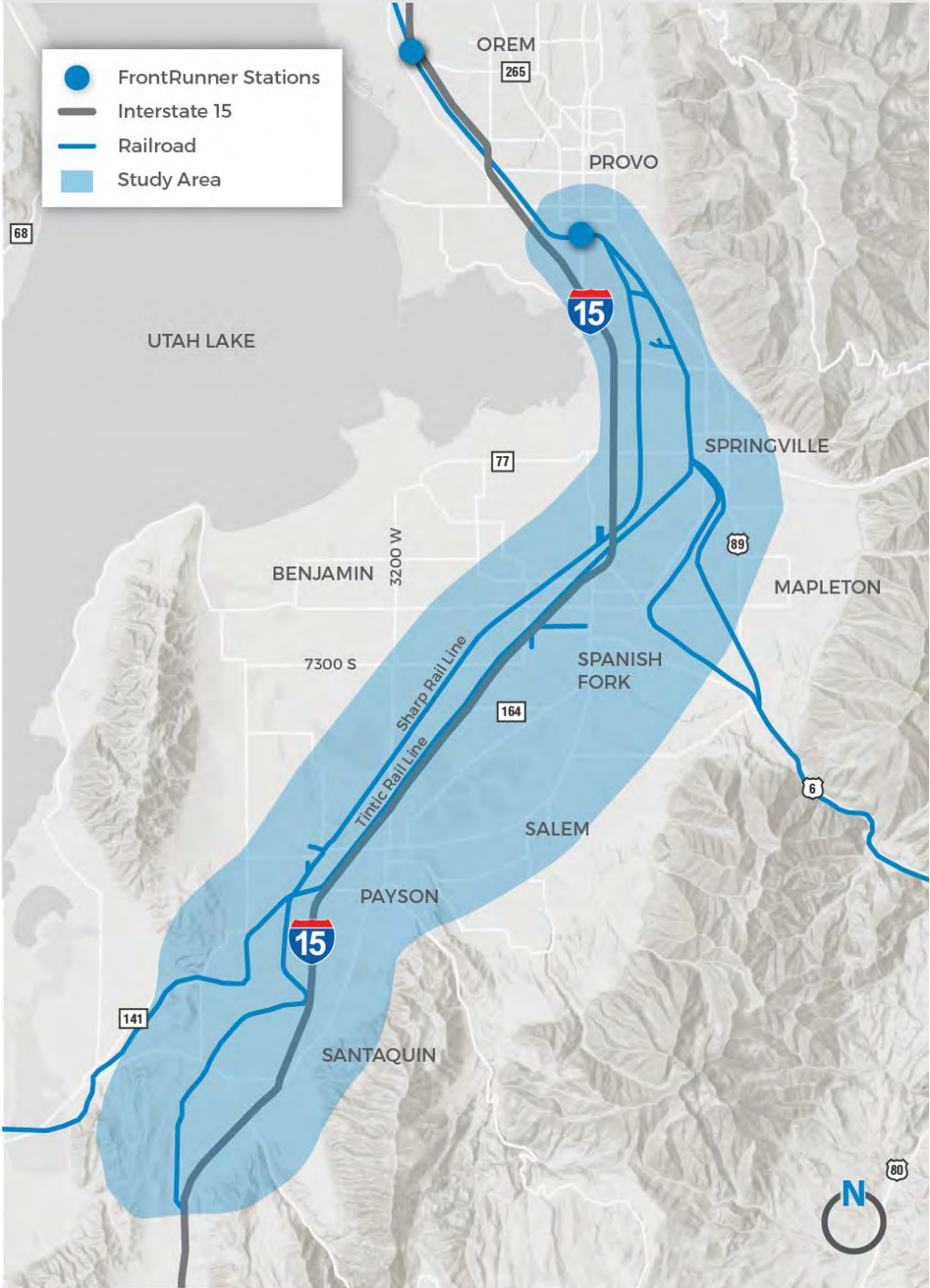


PRESENTERS

- **Mary De La Mare-Schaefer** | Utah Transit Authority
Study Manager/Regional General Manager
- **Shawn Seager** | Mountainland Association of Governments
Director of Regional Planning
- **Megan Waters** | Utah Transit Authority
Community Engagement Manager
- **Claire Woodman** | Parametrix
Consultant Study Manager
- **Shane Marshall** | Horrocks Engineers
Agency Coordination
- **Katie Williams** | Horrocks Engineers
Public Engagement

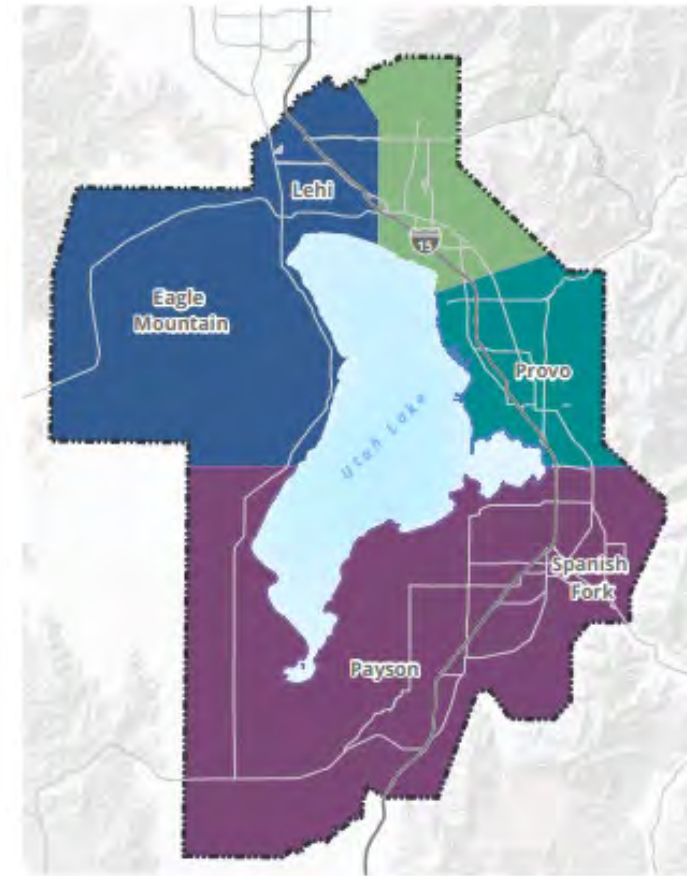
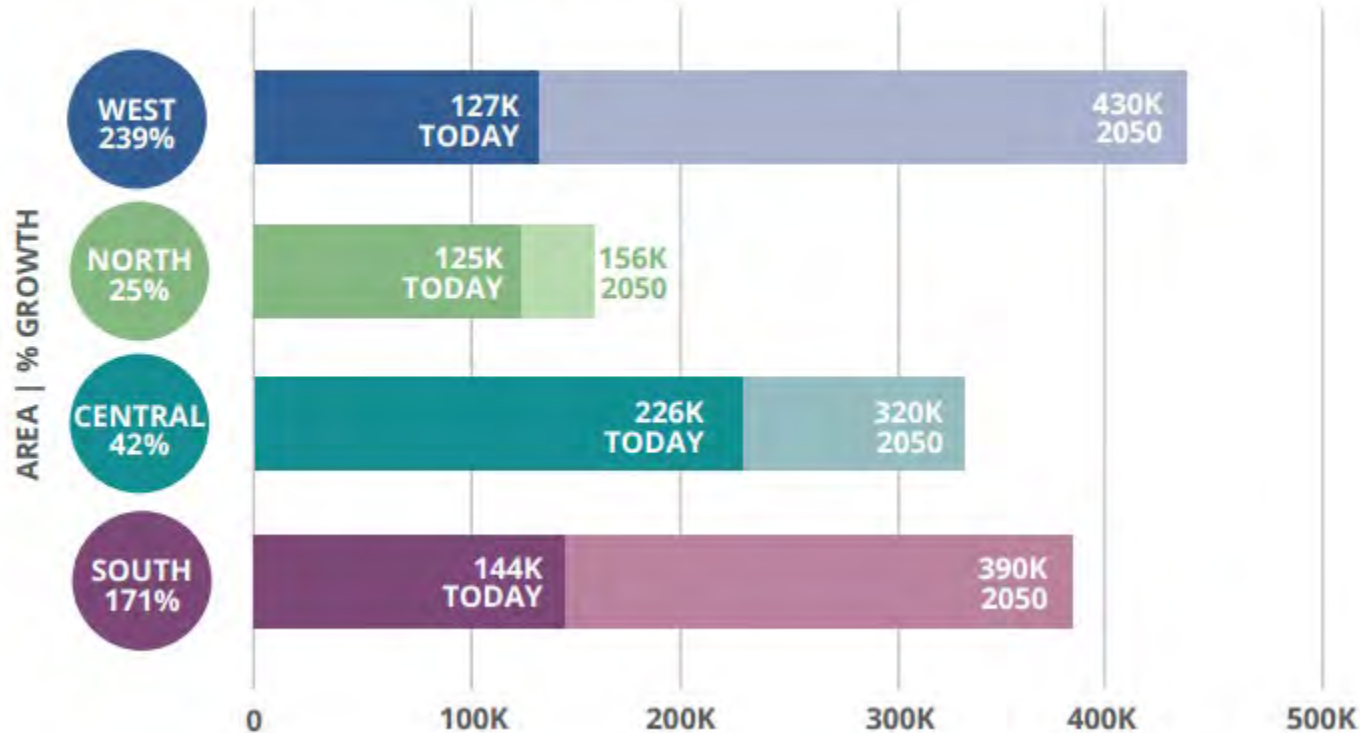


STUDY AREA



POPULATION & EMPLOYMENT GROWTH

POPULATION GROWTH BY SUB-COUNTY AREA



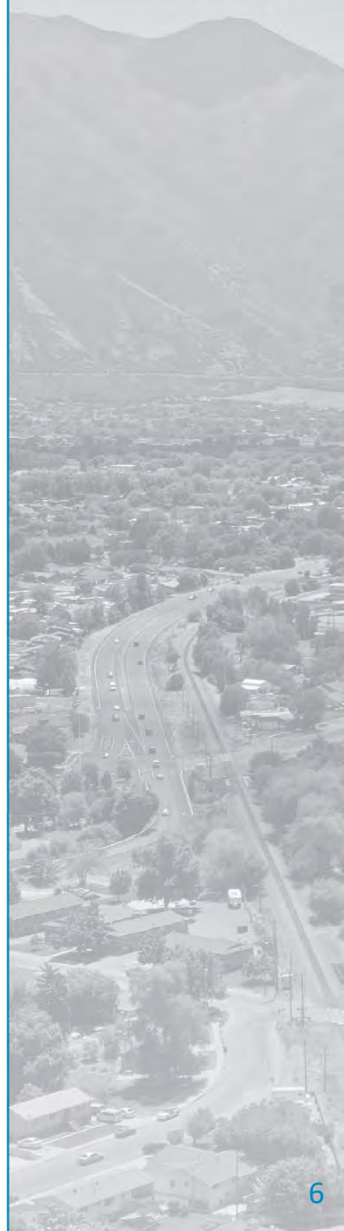
MPO Boundary

- West
- North
- Central
- South



STUDY PURPOSE

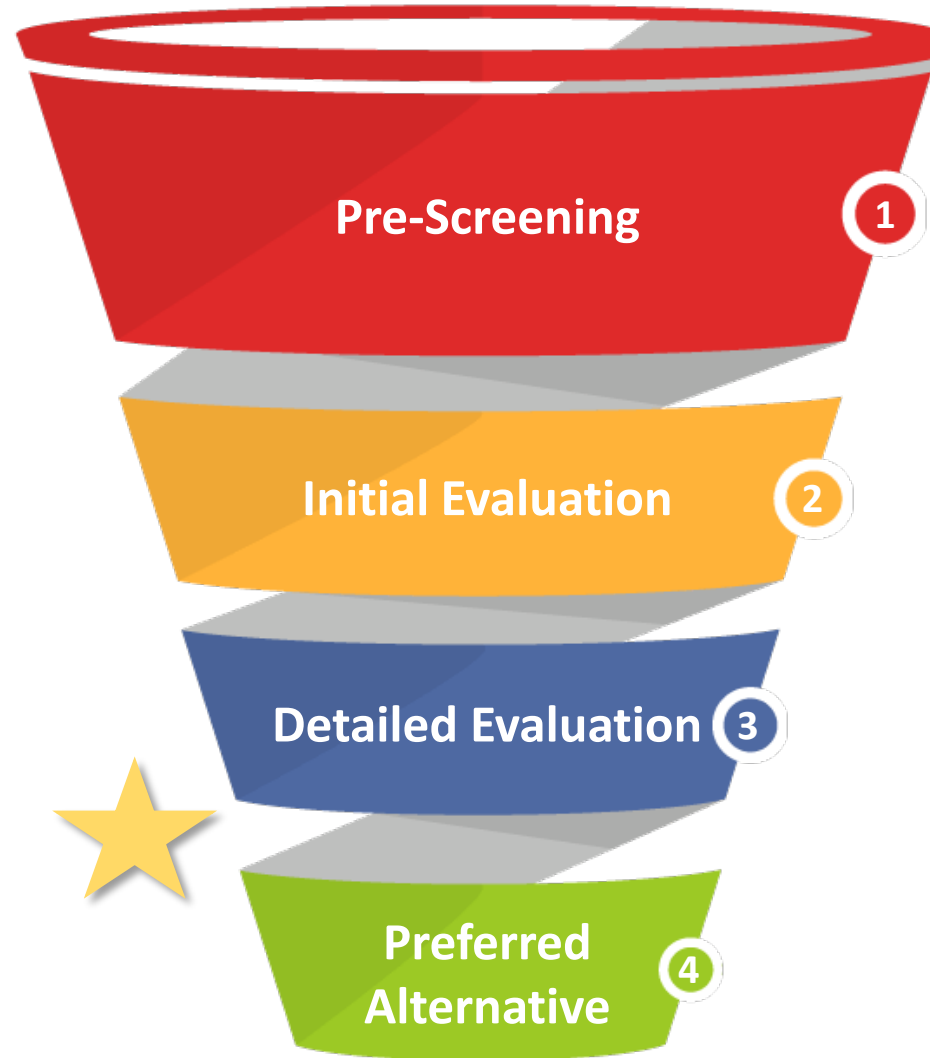
- **Evaluate high-capacity transit improvements** from Provo to Santaquin
- **Select a Locally Preferred Alternative for transit** (alignment and mode) that can be moved into future phases of project development
- **Provide a transparent and collaborative process between all project partners** (Provo, Springville, Mapleton, Spanish Fork, Salem, Payson, Santaquin, Utah County, MAG and UTA)



OVERVIEW

ALTERNATIVE ANALYSIS PROCESS

- This multi-step alternatives evaluation process to determine the long-term preferred solution for providing expanded transit service in southern Utah County from Provo to Santaquin
- The ***Detailed evaluation*** step builds on the high-level screening and provides more quantitative information to inform selection of a Locally Preferred Alternative

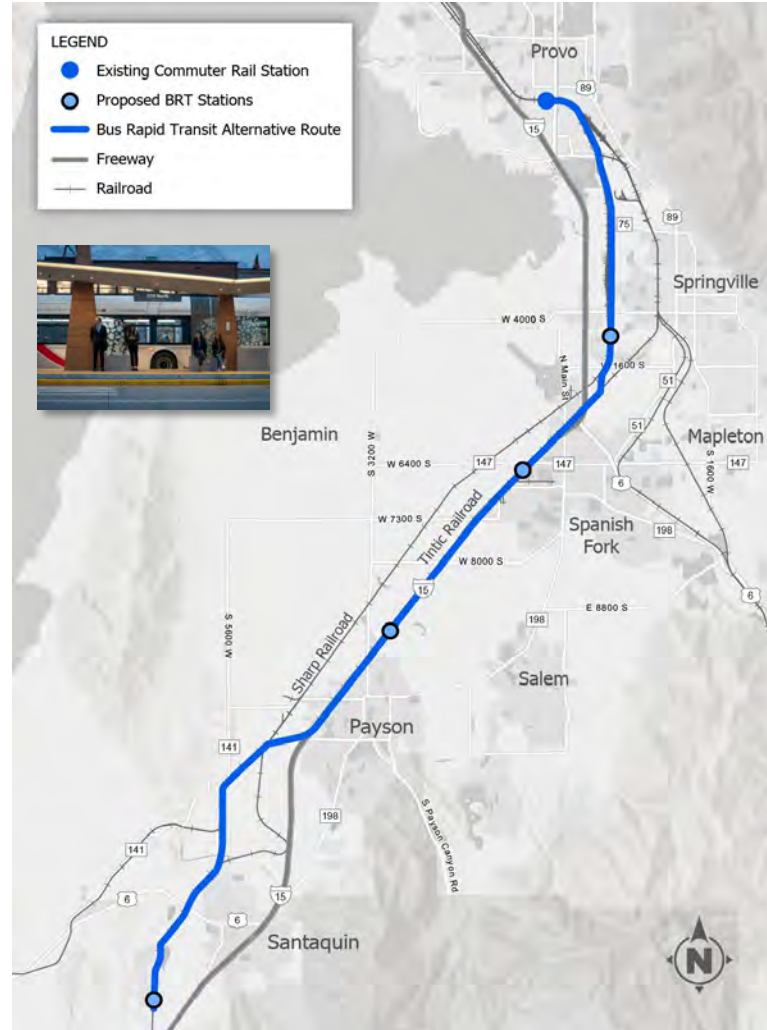


DETAILED RANGE OF ALTERNATIVES

Commuter Rail Alternative



Bus Rapid Transit Alternative



Bus Rapid Transit Design Option Alternative








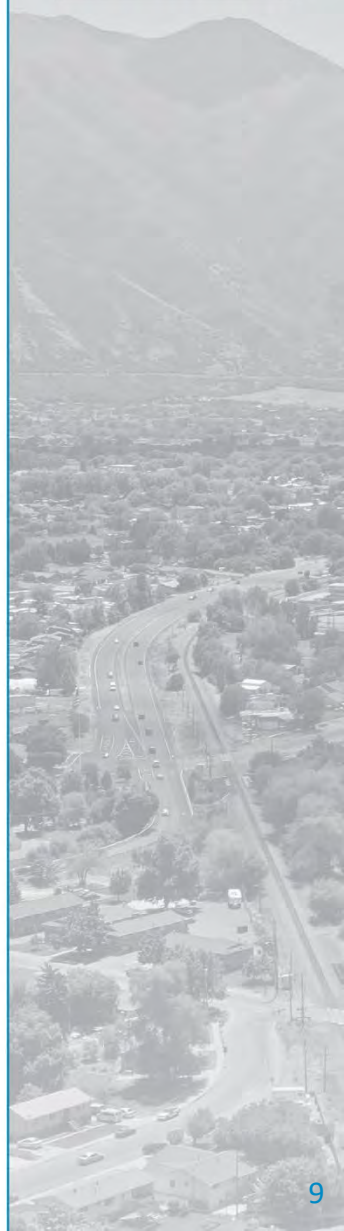
← Commuter Rail Alternative and Bus Rapid Transit Alternative share same alignment/station locations →

Bus Rapid Transit Design Option Alternative developed to reduce costs and impacts

WHAT DID WE LEARN?

SIMILARITIES BETWEEN COMMUTER RAIL AND BUS RAPID TRANSIT (BRT)







-  Transit reliability
-  Transportation system impacts
-  Land use compatibility
-  Transit-oriented development potential
Both alternatives include the same stations
-  Natural and built environmental impacts









WHAT DID WE LEARN?

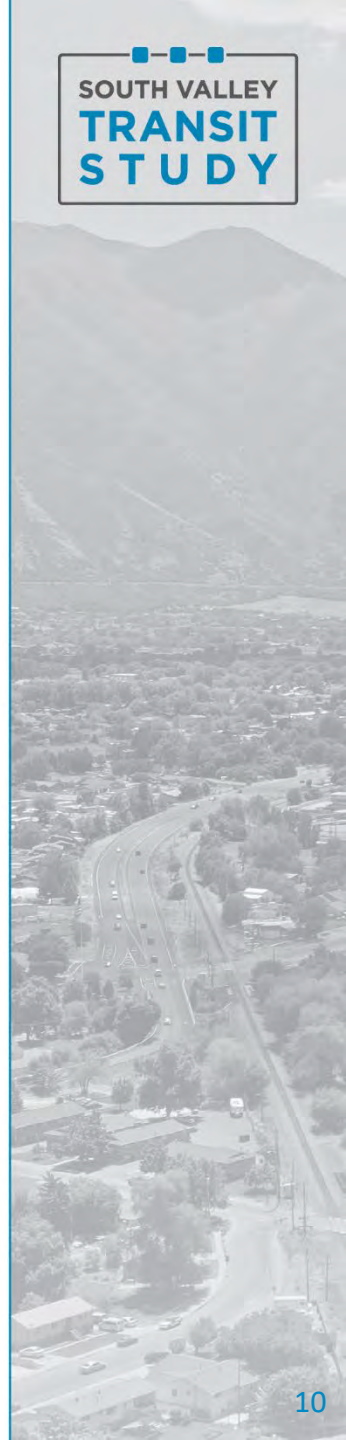
DIFFERENCES BETWEEN COMMUTER RAIL AND BRT

Commuter Rail

-  Regional travel times
-  Ridership
-  Capital costs
-  Operation and Maintenance costs
-  Return on investment
-  Construction complexity

BRT

-  Regional travel times
-  Ridership
-  Capital costs
-  Operation and Maintenance costs
-  Return on investment
-  Construction complexity



HOW IS THE BRT DESIGN OPTION DIFFERENT?

- **Improves performance by:**

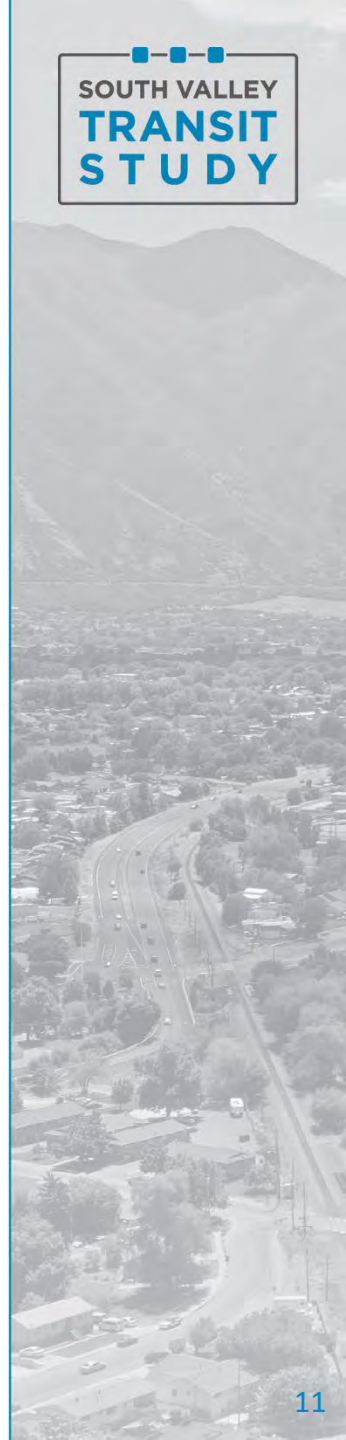


- Reducing capital costs
- Reducing Operations and Maintenance (O&M) cost
- Reducing natural and built environment impacts
- Reducing construction complexity

- **Reduces performance by:**



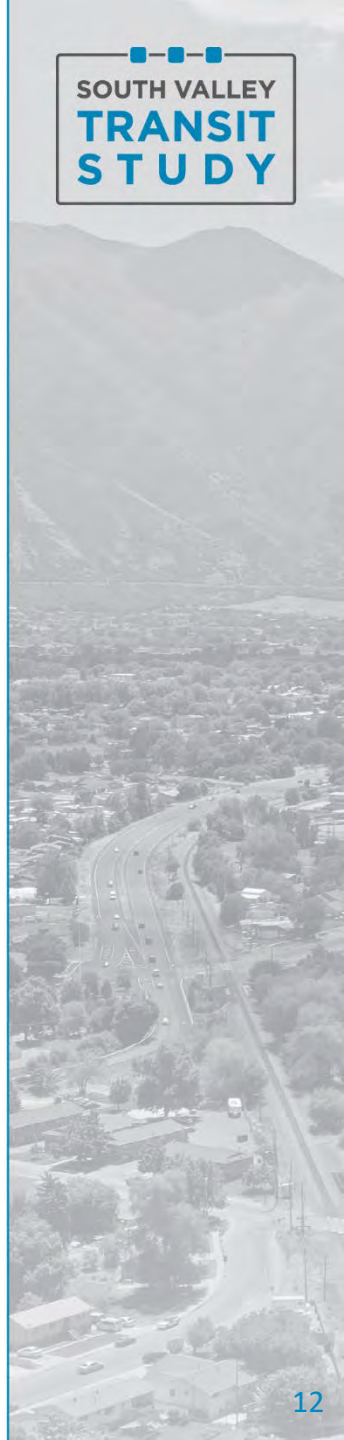
- Increasing travel times
- Reducing ridership
- Less land use compatibility
- Reducing transit-oriented development potential
- Raises the return on investment



PHASING AND IMPLEMENTATION

Phasing and Implementation Considerations

- Provo to Payson is key segment
 - Reduces cost (capital and operation and maintenance)
 - Improves return-on-investment
 - Reduces natural and built environment impacts
- Payson to Santaquin
 - Focuses on identification and preservation of right-of-way
 - Requires evaluation of agricultural considerations and impacts
 - Express bus service will connect Santaquin to commuter rail



PUBLIC INPUT

- 818 comments
- 2,564 website viewers
- 8 events



Public Feedback:

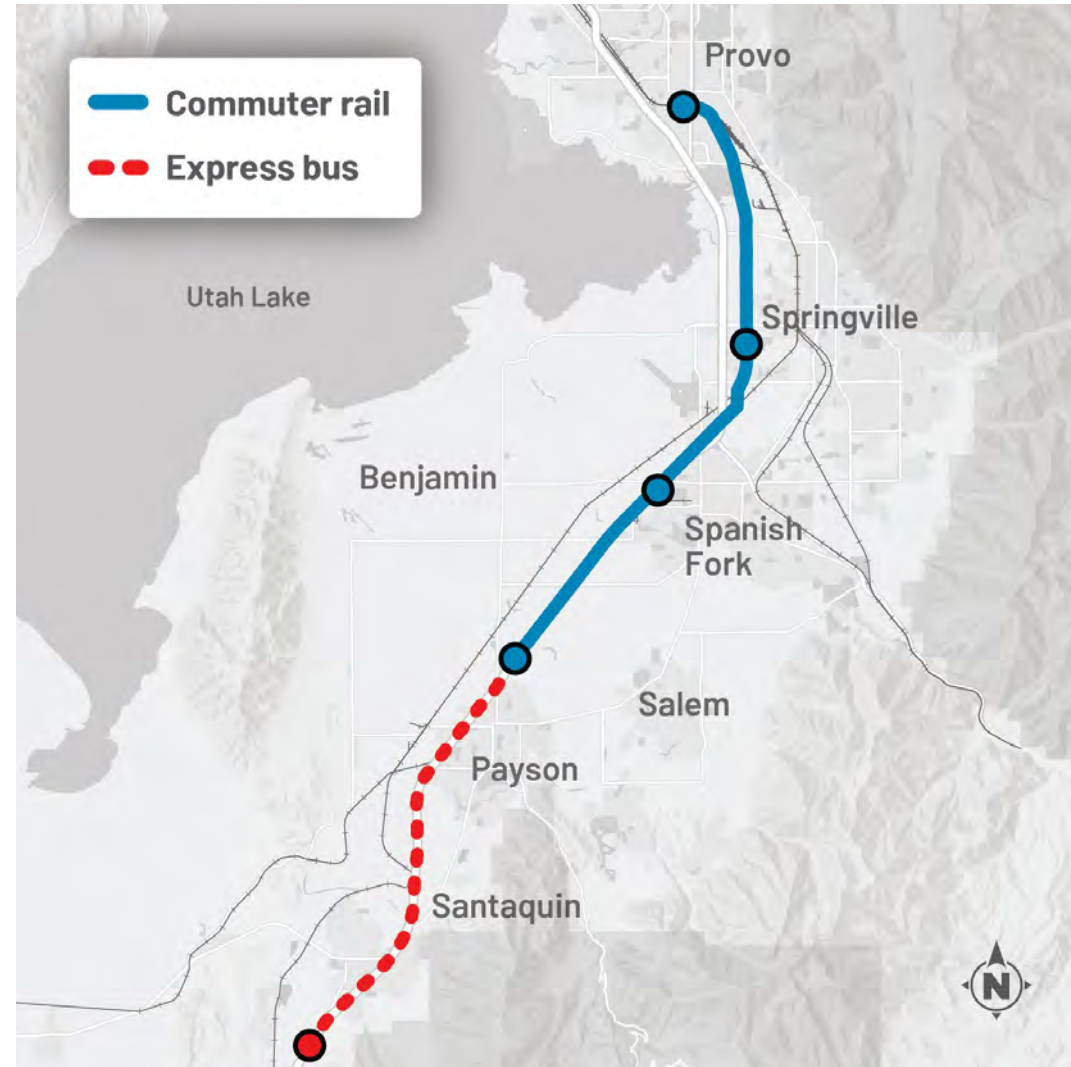
- Majority of respondents support frequent, reliable and affordable service
 - Minority of respondents oppose transit in South Utah County
- Support increased development at station areas
 - E.g., businesses, commercial opportunities and housing
- Strong support for FrontRunner
- Support for localized service to complement FrontRunner



LOCALLY PREFERRED ALTERNATIVE

Proposed Recommendation

- **Commuter Rail**
 - Provo to Payson
- **Express Bus Service**
 - Payson to Santaquin



NEXT STEPS

➤ Oct. and Nov. 2021

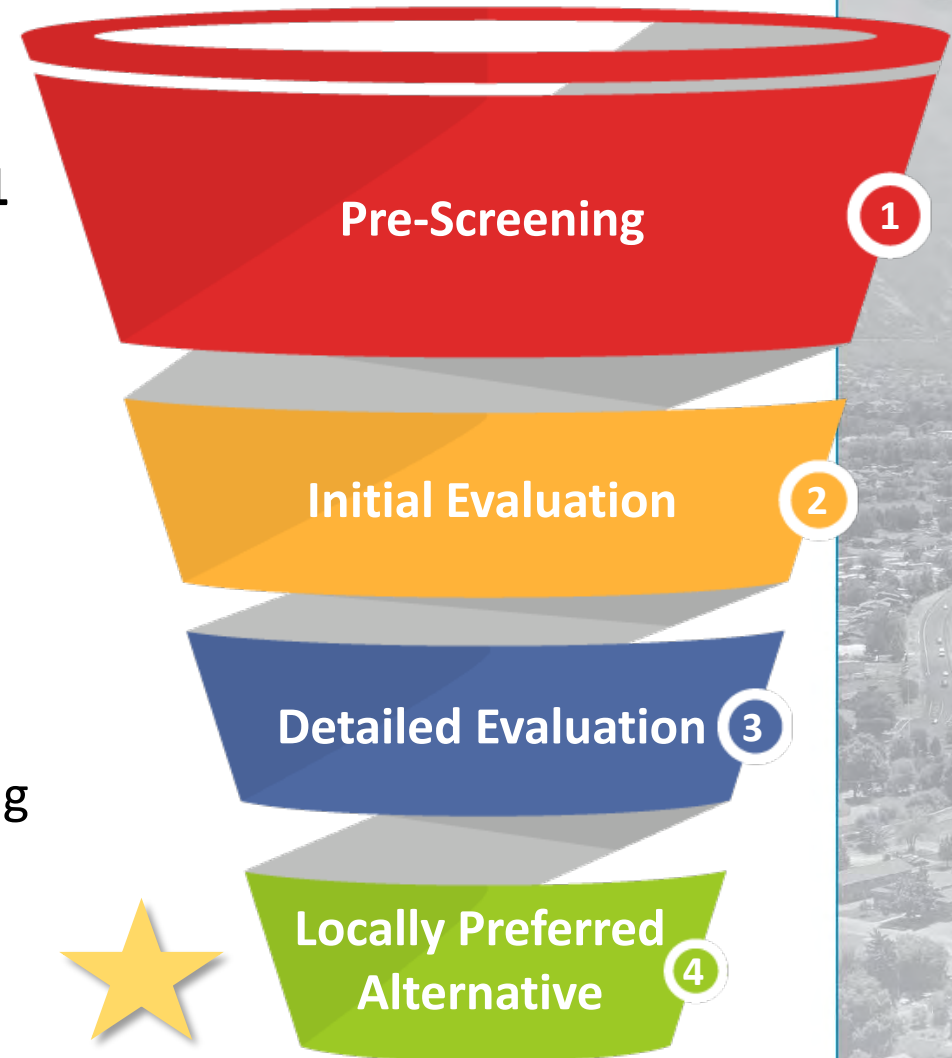
- Gather public input
 - Comment period is open until **Nov. 12, 2021**
- Finalize study

➤ Early 2022

- Environmental study

➤ Project Construction and Implementation

- Exact timing is unknown and based on securing additional funding
- Will learn more as study is advanced to environmental
- Timeframe for major transit projects (RTP)



PUBLIC COMMUNICATION AND COMMENT CHANNELS

Comment period is open until Nov. 12, 2021

HOTLINE: 385-355-3133

EMAIL: southvalleytransit@rideuta.com

WEBSITE: southvalleytransit.com

MAIL: South Valley Transit c/o Horrocks Engineers

2162 West Grove Parkway, Suite 400

Pleasant Grove, UT 84062



QUESTION AND ANSWER

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